ADDENDUM 01 TO BID DOCUMENTS FOR THE

HWY. 384 HYDROLOGIC RESTORATION PROJECT

MAINTENANCE EVENT (CS-21)

CAMERON PARISH, LOUISIANA

May 21, 2015

LOUISIANA COASTAL PROTECTION AND RESTORATION AUTHORITY

ADDENDUM 01 TO BID DOCUMENTS
HWY 384 HYDROLOGIC RESTORATION PROJECT (CS-21)

The clarifications and revisions in this addendum supersede the requirements in the Plans and Specifications dated November 2014. The Successful bidder will be issued a revised set of Plans and Specifications.

I. CPRA Response to Bidder Question

Question: What is the estimated budget for the above mentioned project?

Response: The Engineer's estimate of probable construction cost for this project is $153,165.00.

Question: Can the Contractor use material from other projects if certification of required testing is provided for the proposed fill material?

Response: Contractor may use any material that has undergone required testing and is consistent with the requirements set forth in Section 4.3.1 and 4.3.2 of the Specifications.

Question: Can the contract time be extended?

Response: The contract time has been extended from 30 days to 60 days.

II. Revisions to Specifications

Delete: Sentence 2, Section 4.3.1, TS-4 LEVEE REPAIR

Replace with: Borrow material used for levee embankment construction should consist of homogenously processed naturally occurring or blended earthen materials classified as

CL or CH in accordance with ASTM D 2487. Material should also meet the following requirements:

- Plasticity Index greater than 10,
- contain less than 35% sand,
- an Organic Content of less than 9 percent by dry weight, ASTM D 2974, Method C
- salinity properties amenable to growing turf, and
- be free of deleterious materials which may include organic matter (leaves or paint matter), sticks, branches, trees, logs, stumps, roots, trash, and/or construction debris.

All deleterious material is to be removed from proposed borrow. Proposed borrow shall be tested in conformance with Section 4.3.2.

Delete: Sentence 1 & 2, Section 4.3.2, TS-4 LEVEE REPAIR

Replace with: Prior to levee embankment placement and compaction embankment borrow shall be tested for conformance to these requirements. A minimum of 2 test suites shall be performed and submitted to the Engineer for approval for each material type and source.

The following tests shall be performed:

- Water Content ASTM D 2216,
- Atterberg Limits ASTM D 4318,
- Sand Content ASTM D 1140,
- Organics ASTM D 2974, Method C,
- Soil Classification (USCS Classification) ASTM D 2487, and
- Standard Proctor ASTM D698

If the material is determined to be acceptable, these initial 2 proctor curves will serve as the upper and low bound of the family of curves. These requirements for preconstruction material testing shall be performed on each proposed source of borrow material.

Delete: Sentence 4 & 5, Section 4.3.3, TS 4 LEVEE REPAIR

Replace with: In-place density testing (ASTM D 2922), moisture content (ASTM D 4643), Atterberg Limits (ASTM D 4318), and field classification (ASTM D 2488) shall be performed once per lift on the levee. The test location shall be representative of the area being tested. The percentage of the maximum dry density, the water content as related to the optimum water content using the representative proctor curve shall be determined by experience and judgement from visual soil classification (ASTM D 2488).

The in-place dry density shall be determined by calculation from the nuclear density gauge measurement of wet density and the in-place field moisture content.

Delete: Sentence 5 & 6, Section 5.3, TS-5 DREDGING OF INLET CHANNEL

Replace with: Land-based equipment will be allowed in areas where excavated material is to be placed. Contractor will be required to grade/shape the soil in areas where land-based equipment tracks once all side cast material is in place. It is the Contractor's responsibility to ensure there is no scarring of existing marsh in areas where land-based equipment tracks. The Contractor will bring in borrow material at no expense to the Owner if there is not sufficient material on site to grade/shape scarred marsh.

Add: The inlet channel shall be dredged to a minimum depth of -2.11', and a maximum of -3.11' (NAVD 88). Cross sections on the As-Built drawings must depict these requirements for Acceptance of the Work.

Remove: CS-21-SM-01 Monument Data Sheet in Appedix D of the Specifications

Replace with: Attachment 1 in this Addendum.

Remove: Page 1 (CPRA-IB-1) of the Instruction to Bidder's Document in the Specifications.

Replace with: Attachment 6 of this Addendum.

ADDENDUM 01 TO BID DOCUMENTS
HWY 384 HYDROLOGIC RESTORATION PROJECT (CS-21)

III. Revisions to Plans

The following sheets have been revised from Plans dated November 2014, and shall be replaced with the following:

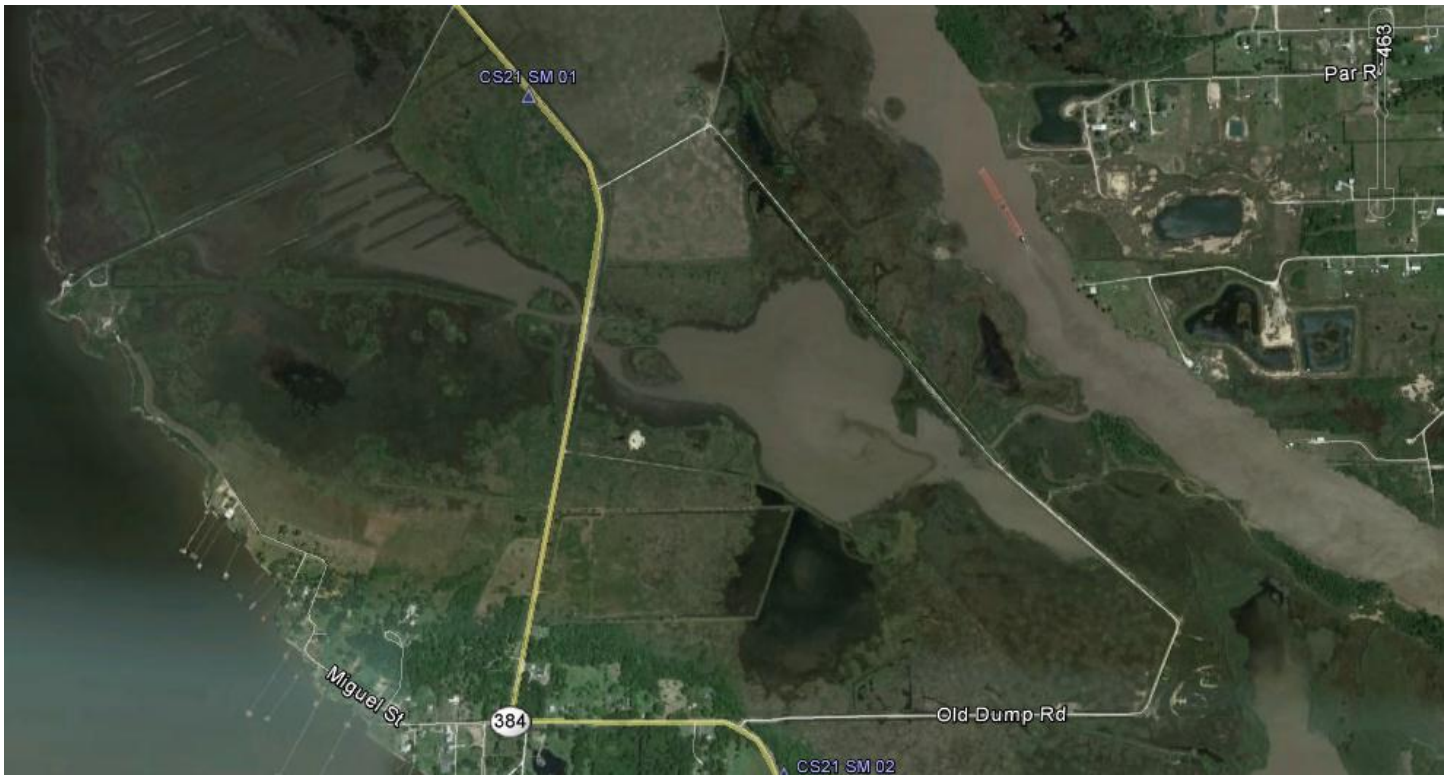
- Replace Sheet 2 with Sheet 2-Rev. 1
- Replace Sheet 3 with Sheet 3- Rev. 1
- Replace Sheet 15 with Sheet 15- Rev. 1
- Add Sheet 26

IV. List of Attachments

- Attachment 1- CS-21-SM-01 Monument Data Sheet
- Attachment 2- Sheet 2-Rev. 1
- Attachment 3- Sheet 3-Rev. 1
- Attachment 4- Sheet 15-Rev. 1
- Attachment 5- Sheet 26
- Attachment 6- Revised Sheet 1 of Instruction to Bidders
- Attachment 7- Pre-Bid Meeting Sign-In Sheet

ATTACHMENT 1

CS-21-SM-01 MONUMENT DATA SHEET



VICINITY MAP Scale: 1" = 2000' Reproduced from Google Earth © 2014

Station Name: CS21-SM-01

Monument Location: From the Sweet Lake Draw Bridge on La. Hwy. 384 crossing the Intracoastal Waterway in Grand Lake, Louisiana, proceed westerly on La. Highway 384 for approximately 4.4 miles to monument at left on west side of highway.

Monument Description: Top Security Style Monument; 2" aluminum cap on 5/8" stainless steel rod driven to refusal, set in 6" PVC sleeve with concrete filled with sand.

Stamping: "384 N" Date: October 2000

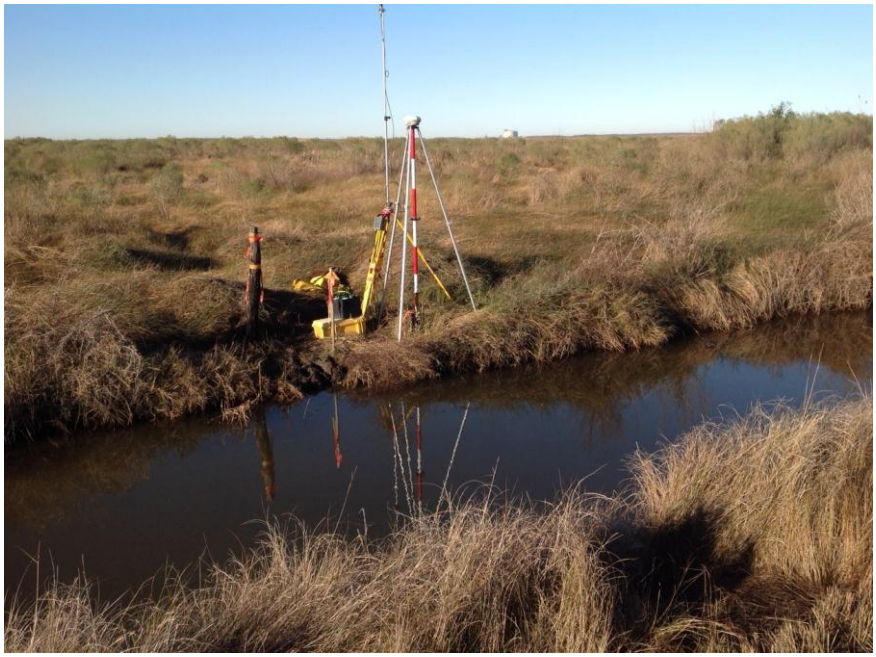
Re-Adjusted January 13, 2014

NAD83 (2011) Epoch 2010 Geo Position
Lat. 30° 02' 34.92835" N
Long. 93° 16' 16.77063" W

NAD83 (2011) Epoch 2010 LSZ (1702) Ft
N= 566,327.88
E= 2,667,690.99

Adjusted NAVD88 Elevation
2.12 Ft (0.646 Mtrs.)

Ellipsoid Height = -26.432m
Geoid12A Height = -27.197m



FOR REFERENCE ONLY
Adjusted NAVD88 Elevation
2.90 Ft (0.883 Mtrs.)

Ellipsoid Height = -26.314m
Geoid99 Height = -27.197m

Adjusted Position Determined by John Chance Land Surveys, Inc. for the Coastal Protection & Restoration Authority of Louisiana

ATTACHMENT 2

SHEET 2-REV. 1

GENERAL NOTES:

1. ALL ELEVATIONS ARE GIVEN IN THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) GEOID 12A U.S. SURVEY FEET. ALL HORIZONTAL COORDINATES ARE GIVEN IN THE NORTH AMERICAN DATUM OF 1983 (NAD83) LOUISIANA STATE PLANE SOUTH ZONE U.S. FEET.
2. THE CONTRACTOR SHALL CALL LOUISIANA ONE CALL AT 1-800-272-3020 A MINIMUM OF 5 WORKING DAYS PRIOR TO THE CONSTRUCTION TO LOCATE EXISTING UTILITIES AT THE PROJECT SITE. THE CONTRACTOR SHALL CONTACT AND COORDINATE WITH PIPELINE COMPANIES WHO HAVE PIPELINES IN THE AREA TO GET PIPELINES MARKED. A MAGNETOMETER SURVEY SHALL BE DONE BY THE CONTRACTOR PRIOR TO ANY DREDGING TAKING PLACE.
3. CONTRACTOR SHALL IDENTIFY AND MARK THE CONSTRUCTION AREA ACCORDING TO THE LINES AND GRADES AS SET FORTH IN THE PLANS. ONLY AFTER FINAL ACCEPTANCE OF THE WORK PERFORMED SHALL THE CONTRACTOR REMOVE THE MARKERS.
4. CONTRACTOR MUST STAY WITHIN FOOT PRINT OF PROPOSED LEVEE. ADJACENT MARSH SHALL NOT BE IMPACTED BY THE LEVEE WORK.
5. EQUIPMENT WILL BE ALLOWED ON SIDECAST MATERIAL PLACEMENT AREAS. CONTRACTOR WILL BE REQUIRED TO GRADE/SHAPE THE SOIL IN AREAS WHERE LAND-BASED EQUIPMENT TRACKS ONCE ALL SIDECAST MATERIAL IS IN PLACE.
6. IMPORTED FILL MATERIAL MUST BE AN ACCEPTABLE LEVEE CLAY MATERIAL (CL OR CH) WITH A PI GREATER THAN 10, LESS THAN 35% SAND, AN ORGANIC CONTENT OF LESS THAN 9% BY DRY WEIGHT (ASTM D2974, METHOD C), AND SALINITY PROPERTIES AMENABLE TO GROWING TURF. ALL DELETERIOUS MATERIAL SHOULD BE REMOVED FROM THE EMBANKMENT BORROW.
5. FILL SHOULD BE PLACED IN 10 TO 12 INCH LOOSE LIFTS. MAXIMUM COMPACTION CRITERIA FOR DRY DENSITY AT LEAST EQUAL TO 90% OF ITS MAXIMUM, AS DETERMINED BY THE STANDARD PROCTOR COMPACTION TEST, ASTM D698, SHOULD BE USED.
6. IN-PLACE DENSITY TESTING (ASTM D 2922), MOISTURE CONTENT (ASTM D 4643), ATTERBERG LIMITS (ASTM D4318), AND FIELD CLASSIFICATION (ASTM D2488) SHALL BE PERFORMED ONCE PER LIFT ON THE LEVEE.
7. ALL COMPLETED LEVEE SECTIONS MUST BE GRADED/SLOPED BY MECHANICAL MEANS.
8. THE CONTRACTOR IS RESPONSIBLE FOR ALL SURVEYS TO BE PERFORMED FOR ACCEPTANCE. ALL SURVEYS MUST BE CERTIFIED BY A PROFESSIONAL LAND SURVEYOR LICENSED BY THE STATE OF LOUISIANA. HORIZONTAL MEASUREMENTS SHALL BE REFERENCED TO STATE PLANE COORDINATES. ELEVATION MEASUREMENTS SHALL BE REFERENCED TO NAVD88.
9. THE CONTRACTOR IS RESPONSIBLE FOR NOTIFYING THE LANDOWNERS PRIOR TO ENTERING THE PROJECT SITE FROM OLD DUMP ROAD. ALTERNATE ACCESS ROUTES ARE TO BE APPROVED BY THE ENGINEER PRIOR TO START OF CONSTRUCTION. LANDOWNER CONTACT INFORMATION IS SHOWN BELOW.

CONTACT NAME	PHONE NUMBER	ALTERNATE PHONE NUMBER
MARK LANNIN	(337)474-1699	(337)532-6245
BUCK STEPHENSON	(337)598-1214	(337)661-7111
ELVIN "BUBBA" WHEAT	(337)526-7461	

BID SUMMARY

ITEM NO.	DESCRIPTION	UNIT	QTY
1	MOB AND DEMOB	LUMP SUM	1
2	STRUCTURE MAINTENANCE	LUMP SUM	1
3	LEVEE REPAIR	LINEAR FOOT	1,174
4	DREDGING OF INLET CHANNEL	LINEAR FOOT	1,132
5	CONSTRUCTION SURVEY	LUMP SUM	1

ESTIMATE OF CONSTRUCTION QUANTITIES

WORK DESCRIPTION	UNIT	QTY
LEVEE EMBANKMENT	CUB. YDS.	1160
DREDGING OF CANAL	CUB. YDS.	1060

BENCHMARK INFORMATION

STATION NAME: CS21-SM-01
STAMPING: "384 N"
NAD83 (2011) EPOCH 2010 GEODETIC POSTION:
LATITUDE: 30° 02' 34.92835"
LONGITUDE: 93° 16' 16.77063"
NAD83 (2011) EPOCH 2010 LSZ (1702) FT:
NORTHING: 566,327.88'
EASTING: 2,667,690.99'
ADJUSTED NAVD88 ELEVATION:
2.12 FT
ELLIPSOID HEIGHT= -26.432 M
GEOID12A HEIGHT= -27.197 M

STATE OF LOUISIANA
BEAU J. TATE
License No. 30990
PROFESSIONAL ENGINEER
IN
CIVIL ENGINEERING
5/21/15

REV.	DATE	DESCRIPTION	BY
1	5/21/15	REVISIONS FOR ADDENDUM 1	TH

ROYAL ENGINEERS & CONSULTANTS, LLC
1231 CAMELLIA BOULEVARD
Lafayette, LOUISIANA 70508

COASTAL PROTECTION AND RESTORATION AUTHORITY
450 LAUREL STREET
BATON ROUGE, LOUISIANA 70801

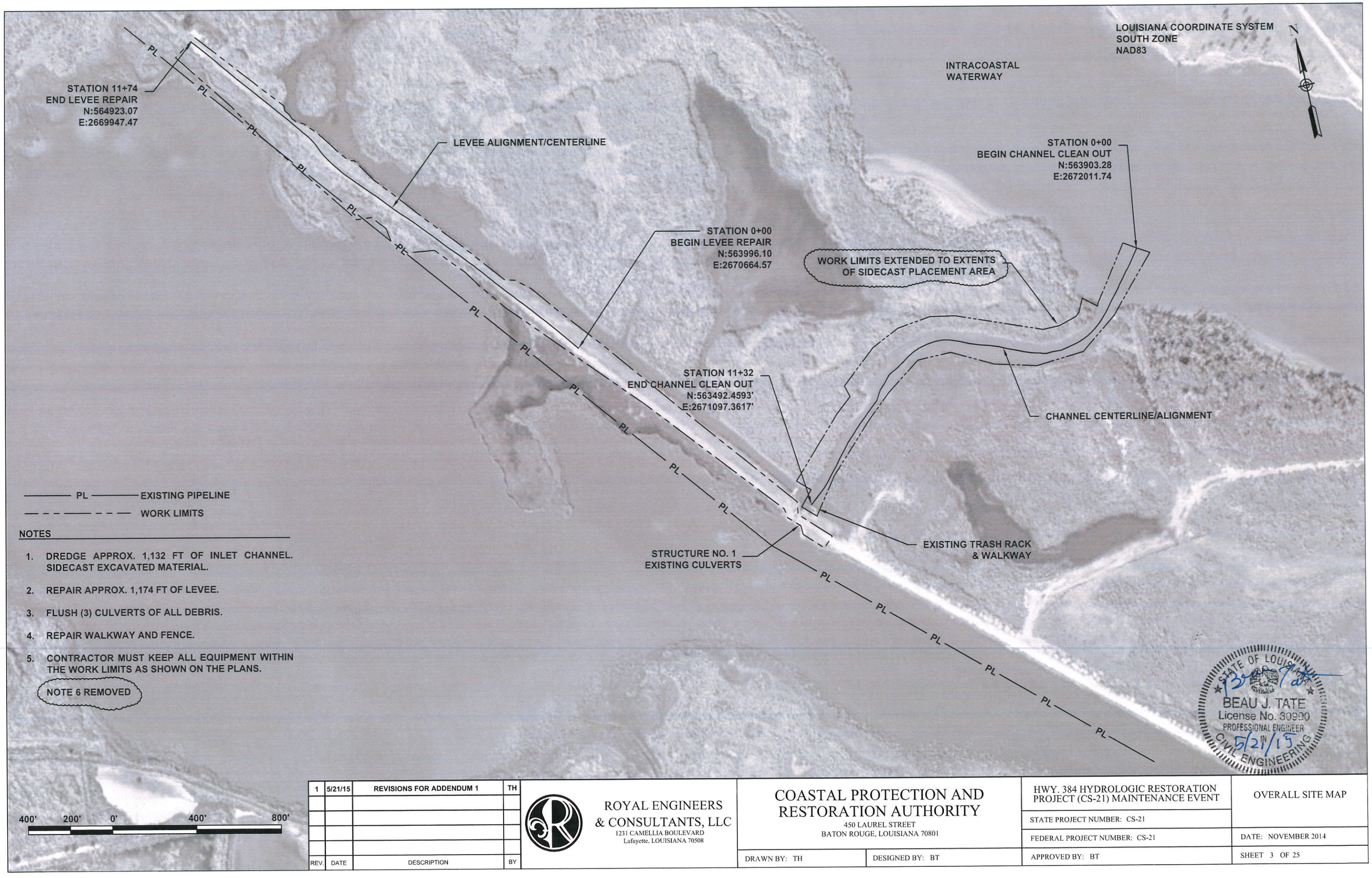
DRAWN BY: TH | DESIGNED BY: BT

HWY. 384 HYDROLOGIC RESTORATION PROJECT (CS-21) MAINTENANCE EVENT
STATE PROJECT NUMBER: CS-21
FEDERAL PROJECT NUMBER: CS-21
APPROVED BY: BT

GENEREAL NOTES
DATE: NOVEMBER 2014

ATTACHMENT 3

SHEET 3-REV.1



NOTES

1. DREDGE APPROX. 1,132 FT OF INLET CHANNEL. SIDECAST EXCAVATED MATERIAL.
2. REPAIR APPROX. 1,174 FT OF LEVEE.
3. FLUSH (3) CULVERTS OF ALL DEBRIS.
4. REPAIR WALKWAY AND FENCE.
5. CONTRACTOR MUST KEEP ALL EQUIPMENT WITHIN THE WORK LIMITS AS SHOWN ON THE PLANS.

NOTE 6 REMOVED

400' 200' 0' 400' 800'

REV.	DATE	DESCRIPTION	BY
1	5/21/15	REVISIONS FOR ADDENDUM 1	TH

STATE OF LOUISIANA
BEAU J. TATE
License No. 30990
PROFESSIONAL ENGINEER
IN
CIVIL ENGINEERING
5/21/15

ROYAL ENGINEERS & CONSULTANTS, LLC
1231 CAMELLIA BOULEVARD
Lafayette, LOUISIANA 70508

COASTAL PROTECTION AND RESTORATION AUTHORITY
450 LAUREL STREET
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DRAWN BY: TH | DESIGNED BY: BT

HWY. 384 HYDROLOGIC RESTORATION PROJECT (CS-21) MAINTENANCE EVENT

STATE PROJECT NUMBER: CS-21

FEDERAL PROJECT NUMBER: CS-21

APPROVED BY: BT

OVERALL SITE MAP

DATE: NOVEMBER 2014

SHEET 3 OF 25

ATTACHMENT 4

SHEET 15-REV. 1

LOUISIANA STATE COORDINATE SYSTEM
SOUTH ZONE NAD83

N

STATION 0+00
BEGIN CHANNEL CLEAN OUT
N:563903.28
E:2672011.74

SIDE CAST MATERIAL

INTRACOASTAL WATERWAY

STATION 11+32
END CHANNEL CLEAN OUT
N:563492.4593'
E:2671097.3617'

0+00
1+00
2+00
3+00
4+00
5+00
6+00
7+00
8+00
9+00
10+00
11+00

CHANNEL CENTERLINE/ALIGNMENT

EXTENTS OF CHANNEL EXCAVATION

PL

STRUCTURE NO. 1
EXISTING CULVERTS

EXISTING TRASH RACK
& WALKWAY

PL — EXISTING PIPELINES

100' 50' 0' 100' 200'

STATE OF LOUISIANA
BEAU J. TATE
License No. 30990
PROFESSIONAL ENGINEER
IN CIVIL ENGINEERING
5/21/15

NOTES:

1. STATION 11+32 IS LOCATED ON THE SOUTHWEST SIDE OF THE HYACINTH FENCE AT THE INVERT OF THE EXISTING CULVERTS. CONTRACTOR SHALL INCLUDE ALL COSTS FOR EXCAVATION OF SEDIMENT AND VEGETATION BEHIND HYACINTH FENCE IN BID ITEM NO. 4 "DREDGING OF INLET CHANNEL".
2. NO EQUIPMENT AND EARTHEN MATERIAL IS ALLOWED OUTSIDE OF THE DESIGNATED WORK LIMITS SHOWN ON SHEET 3 AND THE HATCHED PLACEMENT AREA DESIGNATED ON THIS SHEET. IF LAND-BASED EQUIPMENT IS USED, EQUIPMENT TRACKS WILL NEED TO BE ADDRESSED BEFORE LEAVING THE SITE PER TS-5.

REV.	DATE	DESCRIPTION	BY
1	5/21/15	REVISIONS FOR ADDENDUM 1	TH

ROYAL ENGINEERS & CONSULTANTS, LLC
1231 CAMELLIA BOULEVARD
Lafayette, LOUISIANA 70508

COASTAL PROTECTION AND RESTORATION AUTHORITY
450 LAUREL STREET
BATON ROUGE, LOUISIANA 70801

HWY. 384 HYDROLOGIC RESTORATION PROJECT (CS-21) MAINTENANCE EVENT

SITE PLAN
CHANNEL CLEANOUT

STATE PROJECT NUMBER: CS-21

FEDERAL PROJECT NUMBER: CS-21

DATE: NOVEMBER 2014

DRAWN BY: TH | DESIGNED BY: BT | APPROVED BY: BT

ATTACHMENT 5

SHEET 26

Line No.	Direction	Start Station	End Station	Start Point		End Point		Line No.	Direction	Start Station	End Station	Start Point		End Point	
				Northing	Easting	Northing	Easting					Northing	Easting	Northing	Easting
1	S38° 06' 26"W	0+00.00'	0+05.39'	563903.2800'	2672011.7400'	563899.0380'	2672008.4130'	38	N62° 33' 09"W	4+42.96'	4+65.67'	563746.5820'	2671654.1110'	563757.0490'	2671633.9590'
2	S37° 14' 22"W	0+05.39'	0+18.16'	563899.0380'	2672008.4130'	563888.8700'	2672000.6840'	39	N69° 43' 49"W	4+65.67'	4+88.19'	563757.0490'	2671633.9590'	563764.8510'	2671612.8330'
3	S37° 56' 30"W	0+18.16'	0+31.12'	563888.8700'	2672000.6840'	563878.6500'	2671992.7160'	40	N71° 40' 29"W	4+88.19'	5+11.51'	563764.8510'	2671612.8330'	563772.1820'	2671590.6990'
4	S38° 22' 54"W	0+31.12'	0+41.90'	563878.6500'	2671992.7160'	563870.2000'	2671986.0230'	41	N68° 16' 33"W	5+11.51'	5+34.79'	563772.1820'	2671590.6990'	563780.8010'	2671569.0670'
5	S42° 15' 35"W	0+41.90'	0+49.03'	563870.2000'	2671986.0230'	563864.9240'	2671981.2290'	42	N64° 07' 56"W	5+34.79'	5+60.66'	563780.8010'	2671569.0670'	563792.0870'	2671545.7910'
6	S40° 05' 11"W	0+49.03'	0+53.68'	563864.9240'	2671981.2290'	563861.3680'	2671978.2360'	43	N67° 23' 21"W	5+60.66'	5+82.81'	563792.0870'	2671545.7910'	563800.6040'	2671525.3410'
7	S44° 29' 27"W	0+53.68'	0+63.15'	563861.3680'	2671978.2360'	563854.6120'	2671971.5990'	44	N76° 31' 31"W	5+82.81'	6+01.10'	563800.6040'	2671525.3410'	563804.8650'	2671507.5580'
8	S36° 09' 02"W	0+63.15'	0+75.08'	563854.6120'	2671971.5990'	563844.9770'	2671964.5600'	45	N87° 14' 25"W	6+01.10'	6+19.56'	563804.8650'	2671507.5580'	563805.7540'	2671489.1150'
9	S40° 33' 26"W	0+75.08'	0+84.65'	563844.9770'	2671964.5600'	563837.7090'	2671958.3400'	46	S84° 09' 27"W	6+19.56'	6+37.53'	563805.7540'	2671489.1150'	563803.9250'	2671471.2410'
10	S42° 19' 26"W	0+84.65'	0+95.55'	563837.7090'	2671958.3400'	563829.6470'	2671950.9980'	47	S79° 37' 32"W	6+37.53'	6+59.74'	563803.9250'	2671471.2410'	563799.9250'	2671449.3920'
11	S34° 15' 54"W	0+95.55'	1+06.00'	563829.6470'	2671950.9980'	563821.0130'	2671945.1160'	48	S69° 16' 46"W	6+59.74'	6+76.49'	563799.9250'	2671449.3920'	563794.0000'	2671433.7290'
12	S43° 23' 39"W	1+06.00'	1+16.80'	563821.0130'	2671945.1160'	563813.1650'	2671937.6960'	49	S63° 27' 13"W	6+76.49'	6+90.21'	563794.0000'	2671433.7290'	563787.8700'	2671421.4590'
13	S41° 54' 13"W	1+16.80'	1+28.80'	563813.1650'	2671937.6960'	563804.2310'	2671929.6790'	50	S56° 31' 08"W	6+90.21'	7+09.82'	563787.8700'	2671421.4590'	563777.0520'	2671405.1030'
14	S40° 37' 22"W	1+28.80'	1+38.61'	563804.2310'	2671929.6790'	563796.7840'	2671923.2910'	51	S59° 38' 02"W	7+09.82'	7+30.33'	563777.0520'	2671405.1030'	563766.6810'	2671387.4020'
15	S40° 37' 22"W	1+38.61'	1+49.66'	563796.7840'	2671923.2910'	563788.4017'	2671916.1007'	52	S65° 39' 52"W	7+30.33'	7+46.52'	563766.6810'	2671387.4020'	563760.0110'	2671372.6540'
16	S39° 37' 15"W	1+49.66'	1+65.35'	563788.4017'	2671916.1007'	563776.3170'	2671906.0960'	53	S62° 11' 23"W	7+46.52'	7+54.91'	563760.0110'	2671372.6540'	563756.0940'	2671365.2280'
17	S44° 15' 48"W	1+65.35'	1+82.23'	563776.3170'	2671906.0960'	563764.2270'	2671894.3130'	54	S42° 30' 20"W	7+54.91'	7+79.93'	563756.0940'	2671365.2280'	563737.6480'	2671348.3220'
18	S45° 28' 23"W	1+82.23'	1+97.13'	563764.2270'	2671894.3130'	563753.7760'	2671883.6880'	55	S47° 09' 56"W	7+79.93'	7+96.98'	563737.6480'	2671348.3220'	563726.0580'	2671335.8210'
19	S52° 18' 05"W	1+97.13'	2+15.10'	563753.7760'	2671883.6880'	563742.7900'	2671869.4730'	56	S32° 41' 03"W	7+96.98'	8+23.73'	563726.0580'	2671335.8210'	563703.5470'	2671321.3780'
20	S56° 56' 13"W	2+15.10'	2+27.31'	563742.7900'	2671869.4730'	563736.1260'	2671859.2360'	57	S50° 27' 01"W	8+23.73'	8+46.63'	563703.5470'	2671321.3780'	563688.9660'	2671303.7210'
21	S61° 19' 35"W	2+27.31'	2+40.83'	563736.1260'	2671859.2360'	563729.6410'	2671847.3780'	58	S48° 29' 16"W	8+46.63'	8+60.76'	563688.9660'	2671303.7210'	563679.5980'	2671293.1370'
22	S68° 17' 12"W	2+40.83'	2+53.38'	563729.6410'	2671847.3780'	563724.9990'	2671835.7210'	59	S47° 34' 53"W	8+60.76'	8+71.23'	563679.5980'	2671293.1370'	563672.5340'	2671285.4060'
23	S70° 58' 13"W	2+53.38'	2+66.14'	563724.9990'	2671835.7210'	563720.8380'	2671823.6570'	60	S50° 43' 38"W	8+71.23'	8+89.80'	563672.5340'	2671285.4060'	563660.7820'	2671271.0340'
24	S78° 18' 20"W	2+66.14'	2+80.90'	563720.8380'	2671823.6570'	563717.8460'	2671809.2020'	61	S48° 05' 07"W	8+89.80'	9+03.25'	563660.7820'	2671271.0340'	563651.7950'	2671261.0230'
25	S79° 43' 28"W	2+80.90'	2+96.25'	563717.8460'	2671809.2020'	563715.1080'	2671794.0990'	62	S45° 53' 20"W	9+03.25'	9+29.42'	563651.7950'	2671261.0230'	563633.5820'	2671242.2360'
26	S79° 48' 48"W	2+96.25'	3+13.06'	563715.1080'	2671794.0990'	563712.1340'	2671777.5480'	63	S47° 07' 23"W	9+29.42'	9+44.36'	563633.5820'	2671242.2360'	563623.4130'	2671231.2840'
27	N85° 45' 54"W	3+13.06'	3+24.25'	563712.1340'	2671777.5480'	563712.9600'	2671766.3930'	64	S39° 52' 40"W	9+44.36'	9+59.41'	563623.4130'	2671231.2840'	563611.8650'	2671221.6360'
28	N85° 33' 59"W	3+24.25'	3+34.57'	563712.9600'	2671766.3930'	563713.7580'	2671756.1010'	65	S42° 10' 45"W	9+59.41'	9+74.04'	563611.8650'	2671221.6360'	563601.0260'	2671211.8150'
29	S87° 35' 10"W	3+34.57'	3+47.70'	563713.7580'	2671756.1010'	563713.2050'	2671742.9830'	66	S43° 02' 37"W	9+74.04'	9+88.83'	563601.0260'	2671211.8150'	563590.2180'	2671201.7210'
30	N77° 45' 06"W	3+47.70'	3+58.13'	563713.2050'	2671742.9830'	563715.4180'	2671732.7890'	67	S37° 52' 13"W	9+88.83'	10+04.49'	563590.2180'	2671201.7210'	563577.8550'	2671192.1070'
31	N66° 04' 48"W	3+58.13'	3+65.10'	563715.4180'	2671732.7890'	563718.2430'	2671726.4200'	68	S43° 03' 21"W	10+04.49'	10+27.60'	563577.8550'	2671192.1070'	563560.9650'	2671176.3260'
32	N78° 46' 41"W	3+65.10'	3+71.49'	563718.2430'	2671726.4200'	563719.4870'	2671720.1500'	69	S41° 24' 11"W	10+27.60'	10+50.27'	563560.9650'	2671176.3260'	563543.9640'	2671161.3360'
33	N71° 55' 48"W	3+71.49'	3+78.04'	563719.4870'	2671720.1500'	563721.5190'	2671713.9220'	70	S43° 25' 29"W	10+50.27'	10+65.60'	563543.9640'	2671161.3360'	563532.8300'	2671150.7980'
34	N73° 13' 35"W	3+78.04'	3+86.45'	563721.5190'	2671713.9220'	563723.9460'	2671705.8700'	71	S50° 31' 00"W	10+65.60'	10+79.35'	563532.8300'	2671150.7980'	563524.0840'	2671140.1820'
35	N67° 37' 32"W	3+86.45'	4+02.44'	563723.9460'	2671705.8700'	563730.0310'	2671691.0880'	72	S51° 42' 16"W	10+79.35'	10+95.62'	563524.0840'	2671140.1820'	563514.0055'	2671127.4184'
36	N64° 34' 11"W	4+02.44'	4+22.35'	563730.0310'	2671691.0880'	563738.5830'	2671673.1020'	73	S50° 30' 22"W	10+95.62'	11+15.73'	563514.0055'	2671127.4184'	563501.2110'	2671111.8940'
37	N67° 09' 33"W	4+22.35'	4+42.96'	563738.5830'	2671673.1020'	563746.5820'	2671654.1110'								

STATE OF LOUISIANA
BEAU J. TATE
License No. 30990
PROFESSIONAL ENGINEER
IN
CIVIL ENGINEERING
5/21/15

REV.	DATE	DESCRIPTION	BY
1	5/21/15	REVISIONS FOR ADDENDUM 1	TH

ROYAL ENGINEERS & CONSULTANTS, LLC
1231 CAMELLIA BOULEVARD
Lafayette, LOUISIANA 70508

COASTAL PROTECTION AND RESTORATION AUTHORITY
450 LAUREL STREET
BATON ROUGE, LOUISIANA 70801

DRAWN BY: TH | DESIGNED BY: BT

HWY. 384 HYDROLOGIC RESTORATION PROJECT (CS-21) MAINTENANCE EVENT

STATE PROJECT NUMBER: CS-21

FEDERAL PROJECT NUMBER: CS-21

APPROVED BY: BT

PI POINTS
CHANNEL CENTERLINE

DATE: MAY 2015

SHEET 26 OF 26

ATTACHMENT 6

REVISED SHEET 1 OF INSTRUCTION TO BIDDERS

INSTRUCTIONS TO BIDDERS

COMPLETION TIME:

The Bidder shall agree to fully complete the contract within **Sixty (60)** consecutive calendar days, subject to such extensions as may be granted under Section GP-44 of the General Provisions and acknowledges that this construction time will start on or before the date specified in the written "Notice to Proceed" from the Owner.

LIQUIDATED DAMAGES:

The Bidder shall agree to pay as Liquidated Damages the amount of **Five Hundred Dollars ($500.00)** for each consecutive calendar day for which the work is not complete, beginning with the first day beyond the contract completion date stated on the "Notice to Proceed" or as amended by change order.

ARTICLE 1

DEFINITIONS

1.1 The Bid Documents include the following:

- Advertisement for Bids
- Instructions to Bidders
- Bid Form
- Bid Bond
- General Provisions
- Special Provisions
- Technical Specifications
- Construction Drawings
- Contract Between Owner and Contractor and Performance and Payment Bond
- Affidavit
- User Agency Documents (if applicable)
- Change Order Form
- Recommendation of Acceptance
- Other Documents (if applicable)
- Addenda issued during the bid period and acknowledged in the Bid Form

1.2 All definitions set forth in the General Provisions and the Special Provisions are applicable to the Bid Documents, unless otherwise specifically stated or written.

1.3 Addenda are written and/or graphic instruments issued by the Engineer prior to the opening of bids which modify or interpret the Bid Documents by additions, deletions, clarifications, corrections and prior approvals.

1.4 A bid is a complete and properly signed proposal to do the work or designated portion thereof for the sums stipulated therein supported by data called for by the Bid Documents.

1.5 Base bid is the sum stated in the bid for which the Bidder offers to perform the work described as the base, to which work may be added, or deleted for sums stated in alternate bids.

1.6 An alternate bid (or alternate) is an amount stated in the bid to be added to the amount of the base bid if the corresponding change in project scope or materials or methods of construction described in the Bid Documents is accepted.

1.7 A Bidder is one who submits a bid for a prime Contract with the Owner for the work described in the Bid Documents.

1.8 A Sub-bidder is one who submits a bid to a Bidder for materials and/or labor for a portion of the work.

1.9 Where the word "Engineer" is used in any of the documents, it shall refer to the Prime Designer of the project, regardless of discipline.

ARTICLE 2

PRE-BID CONFERENCE

ATTACHMENT 7
PRE-BID MEETING SIGN-IN SHEET

MANDATORY PRE-BID CONFERENCE
ATTENDANCE RECORD

PROJECT: CS-21 Hwy 384 Hydrologic Restoration Project Maintenance Event
DATE: Monday, May 11, 2015 at 10:00 am
LOCATION: USDA – Natural Resources Conservation Services Field Office, 5417 Gerstner Memorial Drive, Lake Charles

Please Print Clearly

(Sign-In) Printed Name & Signature	Company Name & Address	Contractor License No. & Classification	Phone No. Fax No. & Email	(Sign-Out) Printed Name & Signature
FRANCIS FORTIER	PATRIOT CONST. 600 Jefferson ST. SUITE 820 LAFAYETTE	#53342 HEAVY EQUIP. HIGHWAY & BRIDGES ZNC. & EMB.	Francis@superiorcivil.com	
Ben J. LeBlanc	LeBlanc Marine, LLC P.O. Box 9959 New Iberia, La 70562	56742 Heavy Construction	benleblanc@leblancmarine.com 337-201-3749	Ben J. LeBla.
JOHN F. LEBLANC	DLS, L.L.C. P.O. BOX 309 LYDIA, LA.	UNK	(337) 924-7444 (337) 924-7445 johnedls-energy.com	JOHN F. LEBLANC
Rene Escuriex	Fenstermaker 135 Regency Square Lafayette, LA. 70508	N/A	337-314-0498 337-232-3299 rene@fenstermaker.com	Rene Escuriex

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(Sign-In) Printed Name & Signature	Company Name & Address	Contractor License No. & Classification	Phone No. Fax No. & Email	(Sign-Out) Printed Name & Signature
JEFF Riddle	5409 Hwy 14 E Iowa. LA 70647 Stan's Airboats	52256	JEFF@stansmarshservices.com 337-540-0278	Jeff Riddle
Patrick Hebert	Marshland Equipment P.O. Box 609 Cameron, LA	50874 Heavy Coastal Remediation	775-2838 office pathebert@comtel.net	
Chris Kaha	Javelin Marine 4406 Hwy 14 New Iberia, LA	57127 Spec. Dredging Earthwork Drainage + levees	ckaha@javelin.com (337) 364-5841	Chris Kaha
Edward T. McCain Edward T. McCain	M+M Elec. 864 Hwy 384 L.C., LA. 70607	Heavy Constr.	ET.MMELEC@YAHOO	Edward T. McCain Edward T. McCain

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(Sign-In) Printed Name & Signature	Company Name & Address	Contractor License No. & Classification	Phone No. Fax No. & Email	(Sign-Out) Printed Name & Signature
Lonnie Dunn	Luhr Bros., Inc. P.O. Box 7886 Alexandria LA 71302	1209	318-487-8263 P 318 442-8645 F lonnie@luhralex.com	Lonnie Dunn
Nathan Dondis	PO Box 967 Lake Arthur, LA 70549	40903	337-540-8313 337-205-8519 NATHANDondis@yahoo.com	NATHAN Dondis
Tyler Roy Will T. R.	Wilco Marsh Buggies 1304 Macarthur Ave Harvey, LA 70858	Heavy Construction	337-945-8301 troy@wilcomarshbuggies.com	~~[illegible]~~ Will T. R.
Jody White Jody Roger White	CPRA 635 Cajundome Blvd Lafayette, LA		337-482-0664 jody.white@la.gov	Jody Roger White

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Tory Hebert Tory Hebert	Royal 1231 Camellia Blvd. Lafayette, LA	—	337-456-5351 thebert@royalengineering.net	Tory Hebert Tory Hebert
LOLAND BROUSSARD	USDA-NRCS			
Beau Tate	Royal 1231 Camellia Blvd Lafayette, LA 70508		337 456 5351 btate@royalengineering.net	

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